

A green-tinted architectural sketch of an urban street scene. The drawing shows a street with a sidewalk, a railing, and several trees. In the foreground, a person is walking towards the right, carrying a bag. In the background, another person is walking away from the viewer. The style is a loose, expressive line drawing with a textured, stippled effect. The overall color palette is a monochromatic green, ranging from light to dark tones.

URBAN REGULATING STANDARDS

LAND USE CLASSIFICATIONS

The Design Code for **VICINIA** is developed around The Transect, a system of land classifications described in The Lexicon of the New Urbanism, which incorporates a fine-grained network of lot distinctions. These characteristics follow the natural internal structure of an authentic neighborhood and serve to create the structure of the community of **VICINIA**. This structure is expressed as three urban sectors: Urban Center (T5), General Urban zone (T4) and Reserve/Preserve.

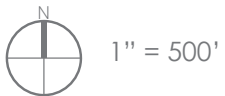


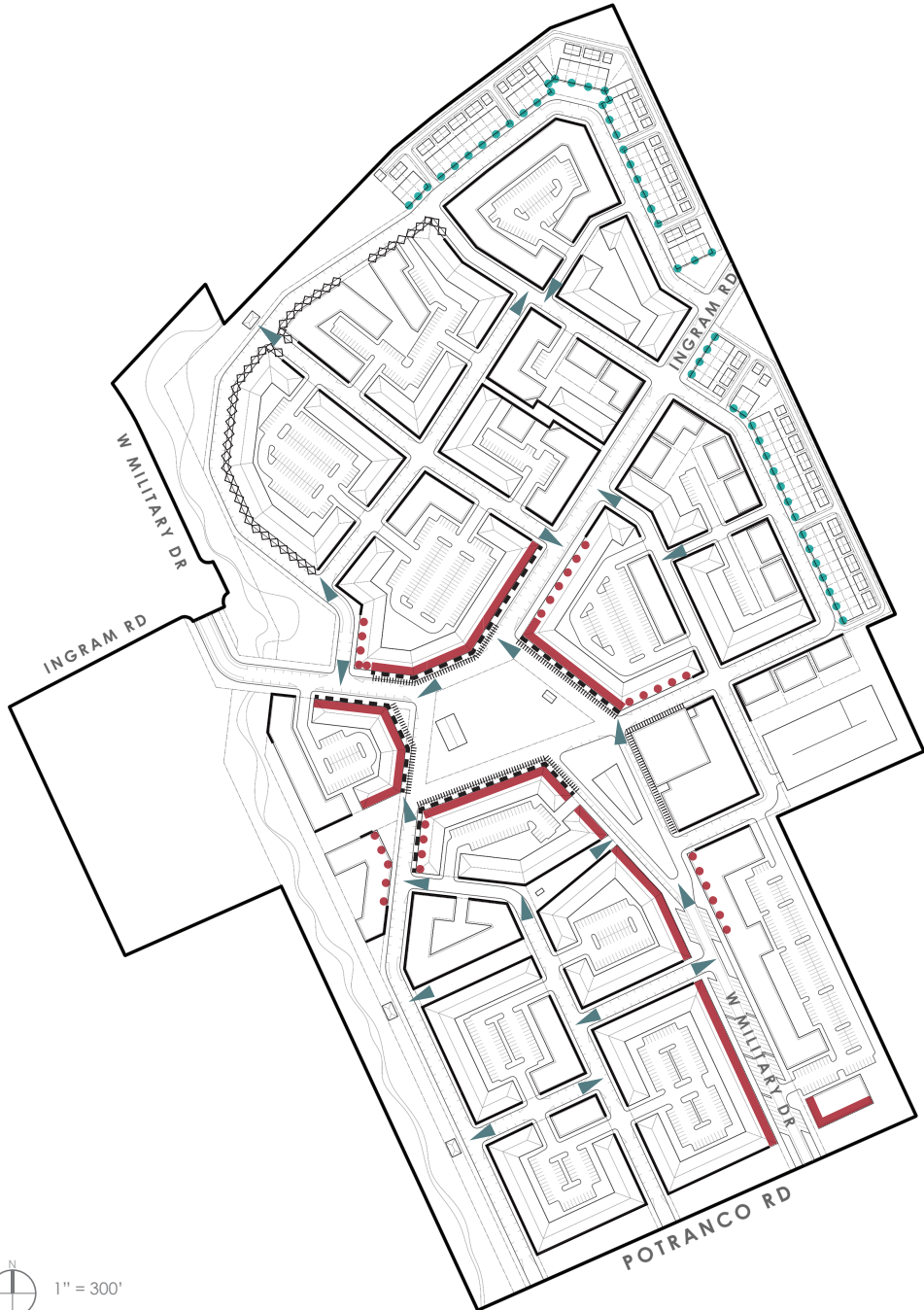
REGULATING PLAN









The Urban Center Zone (T5) is the focus of the neighborhood's civic buildings and social activity. It incorporates retail, workplaces, and more dense residential units, and it connects directly to other parts of the neighborhood through a network of carefully articulated vehicular and pedestrian thoroughfares. As such, it is the densest graining of land subdivision in the neighborhood. The streets are generally designed with formalized on street parking characterized by avenues and main streets. Buildings placed either at or near the right-of-way line, further reinforce the streets edge and public character.

The General Urban Zone (T4) is that element of the transect which focuses principally on residential use with a minimum of other potential uses. Streets and boulevards generally characterize the thoroughfare makeup within the General Urban Zone.

- T5
- T4
- CIVIC USE
- RESERVE
- PRESERVE





-  FRONTAGE BUILDOUT
-  MANDATORY COMMERCIAL / RETAIL
-  RECOMMENDED SHOP FRONT
-  RECOMMENDED ARCADE / GALLERY
-  RECOMMENDED BALCONIES
-  MANDATORY DOORYARD
-  MANDATORY STOOPS
-  TERMINATED VISTA

 1" = 300'

Vicinia

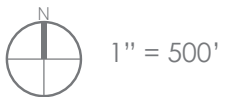
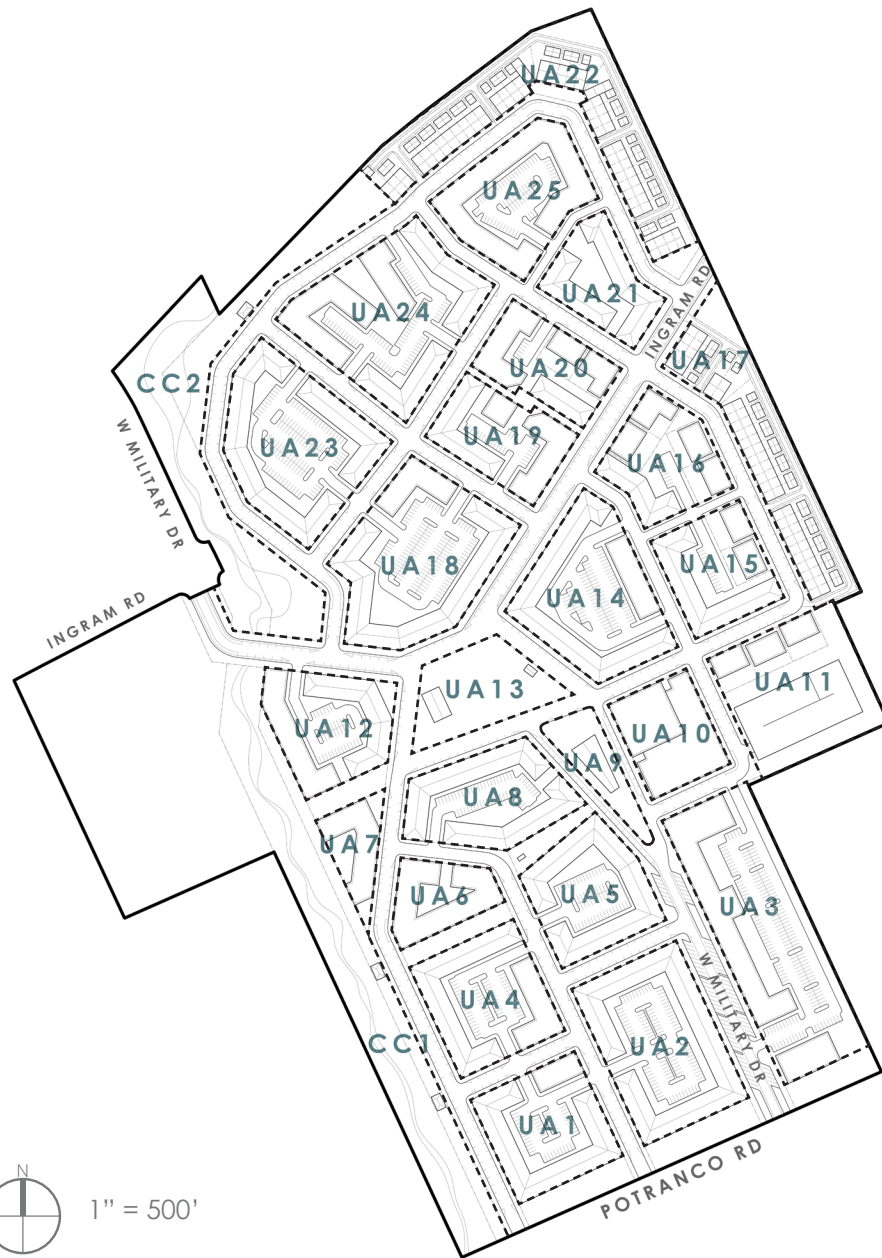
URBAN REGULATION INSTRUCTIONS

Land Classifications	Village Context		Horizontal Placement BTL : Build to Line (Mandatory)
	T5 (Intentionally Left Blank)	T4	
Architectural (Building) Typologies	Commercial/Retail	0-5 0 10	Frontage Non-street Frontages Rear
	Mixed Use (Multifamily)	0-5 0 10	Frontage Non-street Frontages Rear @ Non-Alley
	Mansion Condos	3-5 5 min 10 BTL	Frontage Minimum Side (Left & Right) Rear @ Non-Alley
	Rowhouses/Townhouses (Single Family Attached)	3-5 0 BTL	Frontage Non-street Frontages Rear @ Alley @ Back-out Parking Rear @ Alley @ All other Structures
	Building Use	See Outbuilding Horizontal Placement Instructions Below	
	Building Configuration	All types shall permit residential or commercial use at all stories except for townhouses and mansion condos; see land use classifications above.	
	Civic Uses	Buildings exceeding 2 stories shall incorporate a base, middle, and cap per UDC 204(O)-1	
	Corner Lots	Buildings occurring on corner lots shall be treated as a principal elevation on both frontages.	
	Outbuilding/Garage	Allowed only at townhouses. Outbuildings shall permit living and limited office use (a maximum size of 900 SF).	
	Parking	Automobile storage of adequate size and access shall be provided within the lot in accordance with the city of San Antonio, or as defined in the city of San Antonio UDC and amendments thereafter. Follow UDC 35-208-2.	
General Instructions	Facade	The placement of the façade at the front setback line shall be mandatory unless otherwise shown, showing no more than two corners to the frontage. Follow UDC 35-204 (O)(1)-(4)	
	Fences and Garden Walls	In the absence of building, garden walls and gates shall be built from 0 - 8' from the applicable setback line in accordance with the setback. Follow UDC 35-510 (H)(1)-(4)	
	Dooryard	Dooryards shall be provided as indicated on the urban regulating opportunities and constraints plan. The wall shall be located @ the frontage line in accordance with the "corner lots" view triangle and shall be a height of 2'-6" above the leading edge of the sidewalk @ the frontage line. The first floor shall be elevated above the leading edge of the dooryard a min. of 3'-0"	
	Corner Lots	Buildings on corner lots shall hold clear a view triangle as approved in the San Antonio UDC. See street sections and details.	
	Parking	There shall be a minimum 15' proscenium from the property line per UDC 35-208(P). Parking should be located in the center of the block. The parking and sidewalk system shall be landscaped to provide shade and shelter the street edge. Provide through-block connectors to provide accessibility between parking and building frontages @ intervals not to exceed 150 feet.	
	Outbuilding/Garage @ Terminated Alley Parking	Outbuilding/garage occurring at terminated alley lots can be built @ the frontage line of the terminated lot. Rear alley access @ backout parking and side	
	Porches, Balconies, and Stoops	Porches, balconies, and stoops shall be provided in any one of the combinations shown in the building placement guidelines and in accordance with the Urban Regulating Plan.	
	Main Floor	The first floor level shall be elevated above the grade at the porch or stoop a minimum of 2'-6", except that commercial uses shall be near sidewalk grade.	
	Floor Heights	The first story interior clear height shall be no less than 12' nor more than 16'. All others shall be 9'-10' clear height min.	
	Corner Lots	All lots occurring at block corners shall be a minimum of 4 stories. All lots occurring at block corners shall be 2.5 stories minimum.	
Roofs	Buildings may have flat roofs enclosed by parapets or sloped roofs in accordance with the architectural guidelines.		
Horizontal Placement Instructions	Commercial/Retail	0-5 0 10	Frontage Non-street Frontages Rear
	Mixed Use (Multifamily)	0-5 0 10	Frontage Non-street Frontages Rear @ Non-Alley
	Mansion Condos	3-5 5 min 10 BTL	Frontage Minimum Side (Left & Right) Rear @ Non-Alley
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Vertical Placement Instructions	Facade	The placement of the façade at the front setback line shall be mandatory unless otherwise shown, showing no more than two corners to the frontage. Follow UDC 35-204 (O)(1)-(4)	
	Fences and Garden Walls	In the absence of building, garden walls and gates shall be built from 0 - 8' from the applicable setback line in accordance with the setback. Follow UDC 35-510 (H)(1)-(4)	
	Dooryard	Dooryards shall be provided as indicated on the urban regulating opportunities and constraints plan. The wall shall be located @ the frontage line in accordance with the "corner lots" view triangle and shall be a height of 2'-6" above the leading edge of the sidewalk @ the frontage line. The first floor shall be elevated above the leading edge of the dooryard a min. of 3'-0"	
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	Corner Lots	All lots occurring at block corners shall be a minimum of 4 stories. All lots occurring at block corners shall be 2.5 stories minimum.	
Roofs	Buildings may have flat roofs enclosed by parapets or sloped roofs in accordance with the architectural guidelines.		



URBAN AREA CALCULATIONS

The spreadsheet on the following pages forms a block by block analysis of **VICINIA**. Each block, identified in the diagram to the left, corresponds to a row in the adjacent spreadsheet. Among other information, the spreadsheet provides overall block area, building footprint area, an approximate residential unit count, and an estimate for the available retail area.



Blocks / Bldg	Block Area SF	Block Perimeter	Bldg Frontprint	Stories	Total SF	Efficiency	Rentable SF	Unit SF		Total Units		Unit SF		Total Units		Percentage of Commercial Retail Ground Level	On-Street Parking +/-	Commercial / Retail SF
CC1																		9
CC2																		71
CC3																		11
UA1	111,720	1,358'																31
1			64,308	5	321,540	0.85	273,309	850		321.5		1000		273.3		0		0
UA2	131,845	1,486'																58
1			71,999	4.5	323,996	0.85	275,396	850		324.0		1000		275.4		0.5		36,000
UA3	201,527	1,068'																39
1			51,700	4.75	245,575	0.85	208,739	1250	167.0 Senior Living			1000		208.7		0.25		12,925
2			12,000	2	24,000	0.85	20,400	850	24.0			1000		20.4		1		12,000
UA4	169,500	1,678'																34
1			39,664	4	158,656	0.85	134,858	850		158.7		1000		134.9		0		0
18 Lots @			1,750	2.5			Townhomes											
UA5	92,399	1,200'																40
1			55,268	4.5	248,706	0.85	211,400	850		248.7		1000		211.4		0.5		27,634
UA6	56,372	661'																36
20 Lots @			1,750	2.5			Townhomes											
UA7	72,169	762'																9
1			36,696	2.75	100,914	0.85	85,777	850		100.9		1000		85.8		0.25		9,174
				3.75	137,610	0.85	116,969	850		137.6		1000		117.0		0		0
UA8	75,415	1,153'																28
1			19,177	4	76,708	0.85	65,202	850		76.7		1000		65.2		0		0
2			30,479	3	91,437	0.85	77,721	850		91.4		1000		77.7		1		30,479
				4	121,916	0.85	103,629	850		121.9		1000		103.6		0		0
UA9	32,269	964'																3
1			7,436				Iconic Bldg											
UA10	61,152	994'																36
1			59,267	5	296,335	0.85	251,885											251,885
UA11	124,632	644'																17
1			5,400	3	16,200	0.85	13,770	850		16.2		1000		13.8		0		0
				4	21,600	0.85	18,360	850		21.6		1000		18.4		0		0
2			5,400	3	16,200	0.85	13,770	850		16.2		1000		13.8		0		0
				4	21,600	0.85	18,360	850		21.6		1000		18.4		0		0
3			5,400	3	16,200	0.85	13,770	850		16.2		1000		13.8		0		0
				4	21,600	0.85	18,360	850		21.6		1000		18.4		0		0
UA12	52,354	919'																23
1			16,340	3	49,020	0.85	41,667	850		49.0		1000		41.7		1		16,340
				4	65,360	0.85	55,556	850		65.4		1000		55.6		0		0
2			20,358	3	61,074	0.85	51,913	850		61.1		1000		51.9		0		0
UA13	69,397	1,144'																0
1			4,000	1			Iconic Bldg											
UA14	123,242	1,412'																34
1			20,314	3	60,942	0.85	51,801	850		60.9		1000		51.8		1		20,314
				4	81,256	0.85	69,068	850		81.3		1000		69.1		0		0
2			18,540	3	55,620	0.85	47,277	850		55.6		1000		47.3		0		0
				4	74,160	0.85	63,036	850		74.2		1000		63.0		0		0
3			32,083	3.5	112,291	0.85	95,447	850		112.3		1000		95.4		0.5		16,042
UA15	71,503	1,071'																20
1			31,483	3	94,449	0.85	80,282	850		94.4		1000		80.3		0		0
				4	125,932	0.85	107,042	850		125.9		1000		107.0		0		0
2			5,400	3	16,200	0.85	13,770	850		16.2		1000		13.8		0		0
				4	21,600	0.85	18,360	850		21.6		1000		18.4		0		0
3			5,400	3	16,200	0.85	13,770	850		16.2		1000		13.8		0		0
				4	21,600	0.85	18,360	850		21.6		1000		18.4		0		0

Blocks / Bldg	Block Area SF	Block Perimeter	Bldg Frontprint	Stories	Total SF	Efficiency	Rentable SF	Unit SF	Total Units	Unit SF	Total Units	Percentage of Commercial Retail Ground Level	On-Street Parking +/-	Commercial / Retail SF
UA16	73,031	1,082'											22	
1			16,321	3	48,963	0.85	41,619	850	49.0	1000	41.6	0	0	0
				4	65,284	0.85	55,491	850	65.3	1000	55.5	0	0	0
2			5,400	3	16,200	0.85	13,770	850	16.2	1000	13.8	0	0	0
				4	21,600	0.85	18,360	850	21.6	1000	18.4	0	0	0
3			5,400	3	16,200	0.85	13,770	850	16.2	1000	13.8	0	0	0
				4	21,600	0.85	18,360	850	21.6	1000	18.4	0	0	0
4			5,400	3	16,200	0.85	13,770	850	16.2	1000	13.8	0	0	0
				4	21,600	0.85	18,360	850	21.6	1000	18.4	0	0	0
5			5,400	3	16,200	0.85	13,770	850	16.2	1000	13.8	0	0	0
				4	21,600	0.85	18,360	850	21.6	1000	18.4	0	0	0
UA17	70,000	1,015'											24	
28 Lots @			2,375	2.5	Townhomes									
UA18	149,970	1,554'											44	
1			32,984	3	98,952	0.85	84,109	850	99.0	1000	84.1	1	32,984	0
				4	131,936	0.85	112,146	850	131.9	1000	112.1	0	0	0
2			32,873	3	98,619	0.85	83,826	850	98.6	1000	83.8	0	0	0
				4	131,492	0.85	111,768	850	131.5	1000	111.8	0	0	0
3			13,244	4	52,976	0.85	45,030	850	53.0	1000	45.0	0	0	0
UA19	69,750	1,110'											27	
1			18,600	4	74,400	0.85	63,240	850	74.4	1000	63.2	0	0	0
				3	14,700	0.85	12,495	850	14.7	1000	12.5	0	0	0
2			4,900	4	19,600	0.85	16,660	850	19.6	1000	16.7	0	0	0
				3	62,820	0.85	53,397	850	62.8	1000	53.4	0	0	0
3			20,940	4	83,760	0.85	71,196	850	83.8	1000	71.2	0	0	0
UA20	64,725	1,080'											25	
1			18,240	3	54,720	0.85	46,512	850	54.7	1000	46.5	0	0	0
				4	72,960	0.85	62,016	850	73.0	1000	62.0	0	0	0
2			5,400	3	16,200	0.85	13,770	850	16.2	1000	13.8	0	0	0
				4	21,600	0.85	18,360	850	21.6	1000	18.4	0	0	0
3			5,400	3	16,200	0.85	13,770	850	16.2	1000	13.8	0	0	0
				4	21,600	0.85	18,360	850	21.6	1000	18.4	0	0	0
4			10,050	3	30,150	0.85	25,628	850	30.2	1000	25.6	0	0	0
				4	40,200	0.85	34,170	850	40.2	1000	34.2	0	0	0
UA21	58,594	1,021'											23	
1			9,289	3	27,867	0.85	23,687	850	27.9	1000	23.7	0	0	0
				4	37,156	0.85	31,583	850	37.2	1000	31.6	0	0	0
2			16,265	3	48,795	0.85	41,476	850	48.8	1000	41.5	0	0	0
				4	65,060	0.85	55,301	850	65.1	1000	55.3	0	0	0
3			15,488	2	30,976	0.85	26,330	850	31.0	1000	26.3	0	0	0
				3	46,464	0.85	39,494	850	46.5	1000	39.5	0	0	0
UA22	97,500	1,356'											26	
40 Lots @			2,375	2.5	Townhomes									
UA23	136,385	1,463'											52	
1			17,064	4	68,256	0.85	58,018	850	68.3	1000	58.0	0	0	0
				4	89,056	0.85	75,698	850	89.1	1000	75.7	0	0	0
2			22,264	3	84,378	0.85	71,721	850	84.4	1000	71.7	0	0	0
				3	84,378	0.85	71,721	850	84.4	1000	71.7	0	0	0
UA24	152,492	1,596'											39	
1			19,674	4	78,696	0.85	66,892	850	78.7	1000	66.9	0	0	0
				3	81,048	0.85	68,891	850	81.0	1000	68.9	0	0	0
2			27,016	3	9,228	0.85	7,844	850	9.2	1000	7.8	0	0	0
				3	9,228	0.85	7,844	850	9.2	1000	7.8	0	0	0
3			3,076	3	69,447	0.85	59,030	850	69.4	1000	59.0	0	0	0
				3	69,447	0.85	59,030	850	69.4	1000	59.0	0	0	0
UA25	99,050	1,266'											24	
1			7,120	3	21,360	0.85	18,156	850	21.4	1000	18.2	0	0	0
				4	28,480	0.85	24,208	850	28.5	1000	24.2	0	0	0
2			15,562	3	46,686	0.85	39,683	850	46.7	1000	39.7	0	0	0
				4	62,248	0.85	52,911	850	62.2	1000	52.9	0	0	0
3			34,865	2	69,730	0.85	59,271	850	69.7	1000	59.3	0	0	0
				3	104,595	0.85	88,906	850	104.6	1000	88.9	0	0	0
Grand Totals			1,065,872		3,761,285 Min Story		3,197,092		3,386 Min		2,945 Min		805	465,776 Min
					3,997,802 Max Story		3,281,105		3,782 Max		3,281 Max			

Block Area 2,416,993 SF
 Acres 55 ACRES
 Townhomes 106 LOTS

Note: .25 increments in the Stories means a portion would be dedicated to Retail. Round up to the next highest number for total stories.

TOD CLASSIFICATION

The TOD shall consist of two (2) subdistricts known as the “TOD Core” (TOD-C”) AND THE “TOD Periphery” (“TOD-P”).

TOD-C

All areas within one-quarter (1/4) of a mile of a transit station or major bus boarding location shall be classified as “TOD-C.” The transit station is identified with a red circle in the illustration to the left.

TOD-P

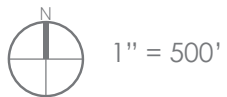
All areas between one-quarter (1/4) of a mile and one-half (1/2) of a mile from a transit station or a major bus boarding location shall be classified as “TOD-P.” No land area shall be zoned “TOD-P” unless it adjoins an area zoned “TOD-C.”



BUILDING TYPES

The majority of structures within **VICINIA** are identified as being mixed-use buildings. These are generally comprised of predominately residential units with the possibility of ground-floor retail. In addition to the mixed-use buildings, sites have been identified for both a senior living component as well as an office building, both along the W Military Dr. extension. There are several civic structures throughout the community and a commercial structure located within the town plaza. Mansion condos and townhouses are located along the outer edge of **VICINIA** to transition the development into the surrounding neighborhoods.

-  TOWNHOUSE
-  MANSION CONDO
-  MIXED-USE
-  COMMERCIAL
-  OFFICE
-  SENIOR LIVING
-  CIVIC



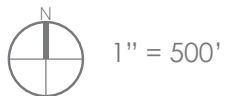
BUILDING HEIGHTS

The illustration to the left conveys the conceptual ideas for building heights in **VICINIA**. Buildings immediately surrounding the town square, as well as those fronting Potranco Rd., have the greatest height within the project. As you move both north and east through **VICINIA**, building heights are gradually reduced in order to better fit within the surrounding neighborhood context.

The concepts presented in this illustration do not relieve the developers from compliance with the mandatory FAR calculations listed within the San Antonio UDC.



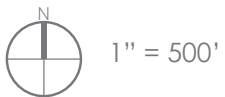
- SPECIAL
- 1 FLOOR
- 1-2 FLOORS
- 2 FLOORS
- 2-3 FLOORS (TOWNHOUSES 2.5 TYPICAL)
- 3 FLOORS
- 3-4 FLOOR
- 4 FLOORS
- 4-5 FLOORS
- 5 FLOORS

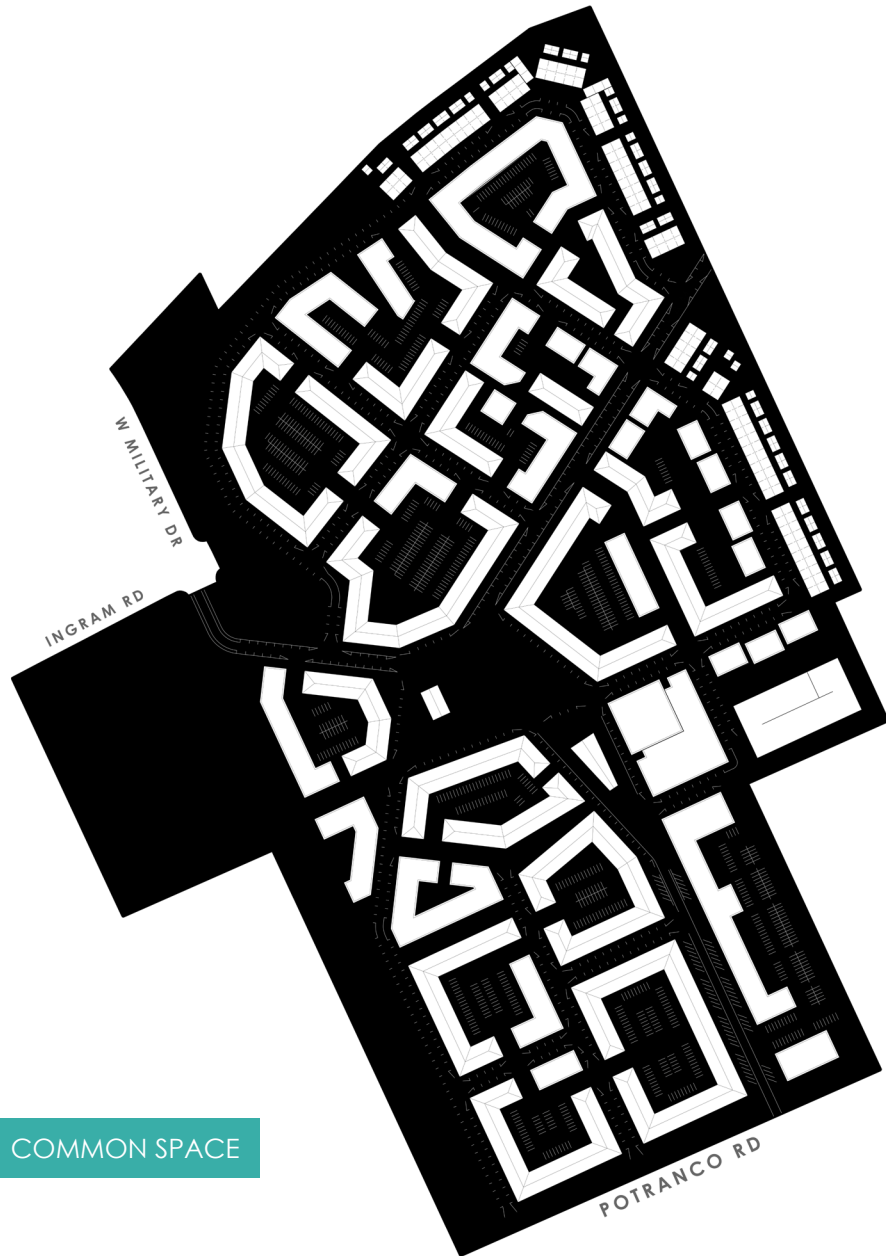


CIVIC SPACE

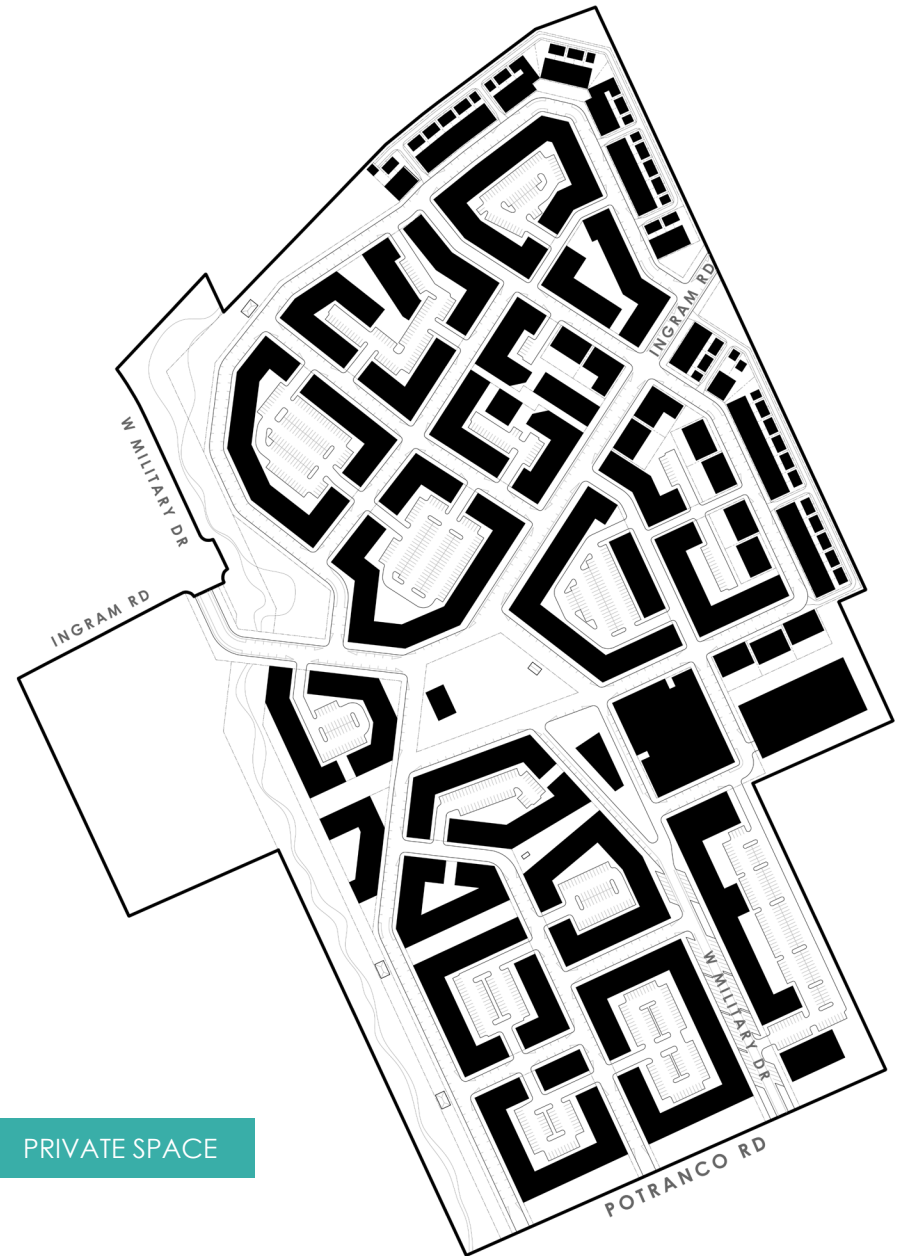
VICINIA includes an assortment civic spaces dispersed throughout the community. The civic spaces in **VICINIA** are all pedestrian oriented, outdoor spaces that are accessible to and designed for the use of the general public. The designated areas are of various sizes and uses. They range from intimate hardscaped pedestrian plazas to expansive green spaces with recreational opportunities.

- PARK
- PLAZA
- POCKET PLAZA
- PLAYGROUND
- PEDESTRIAN PASSAGE












COMMON SPACE

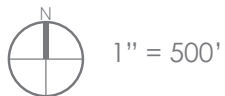


PRIVATE SPACE

CHARACTER DISTRICTS

VICINIA is about a great new place. A place envisioned as sensitive to its distinctive indigenous Texas context. A place that achieves mobility without dependence solely on the automobile, but within a variety of modes including pedestrian, bicycles, and intermodal transit; inhabited by a diverse, social, economic, and age-varied citizenry. A place to be experienced. A place that relies on a new paradigm, built essentially from scratch. A place structured through an architecture of enduring interest and beauty that assembles the public realm through the incorporation of plazas, parks and active engaging street life, with uses above the ground floor punctuated by a mix of uses including residential lofts for sale and for rent. The village is circumscribed by six character districts each with its own distinct attractiveness. An architecture that demands excellence in its design and is uniquely suited to the character defined in each district, each enhanced with a distinct color palette, materials, smells, sounds and interactions.

-  ENTRY SEQUENCE DISTRICT
-  PLAZA DISTRICT
-  CIVIC CONSERVATION DISTRICT
-  MEWS DISTRICT
-  THE FINE GRAINED MULTIUSE DISTRICT
-  THE EDGE DISTRICT
-  RESERVED





ENTRY SEQUENCE DISTRICT

The Entry Sequence role is that of greeting visitors to **VICINIA** at existing adjacent arterials Potranco Road, Military Drive, and the extension of Ingram. As gateways, it is important to announce the interest-creating activity that enhances an engaging experience to all where residents and visitors informally meet. The uses along this sequence will include commercial, retail and living of all sorts, primarily on the ground floor. It is, however, likely that a complete mix of uses will occur above the street. Each district should reflect uniqueness. In particular, this district may identify with a series of architectural elements including towers, arcades, landscaping that enhances the function of the urban space, along with outdoor engagement such seating kiosks etc. As such, it will share some of the highest densities experienced in **VICINIA**.

PLAZA DISTRICT

The Plaza district is the heart of **VICINIA** where everything comes and goes. It is the social corridor with similar densities to the Entry District. It will be the liveliest and most engaging in all of **VICINIA**, situated within a five-minute walk of the entire village where transit systems will embark and disembark 24-7. With all of these uniqueness's, it is the most appropriate place for the positioning of civic uses.



CIVIC CONSERVATION DISTRICT

Of all the predevelopment conditions presented by the site, perhaps the most significant is the drainage way located on the north and west of **VICINIA**. In its place will be a bio swale with trails, bridges, engaging structures, fountains and of course indigenous plantings which will add context to **VICINIA**. Views toward this naturalist swale will help to celebrate the uniqueness of San Antonio and benefit the residents of **VICINIA**.

MEWS DISTRICT

Breaking down blocks into more sensitive scaled urban areas is done in part by the introduction of a series of passages referred to here as "Mews". Historically, mews were used as service corridors and later converted into rich spines of special lofts, shops and restaurants. These have become some of the best places in older settlements. In **VICINIA** these are strictly pedestrian and intended to support artisan activities. The at-grade promenade is intended to support, in addition to living; dining, vending, and artist engagement. Intended to be intimately scaled and serving to reinforce vistas both from outside in, and inside out.

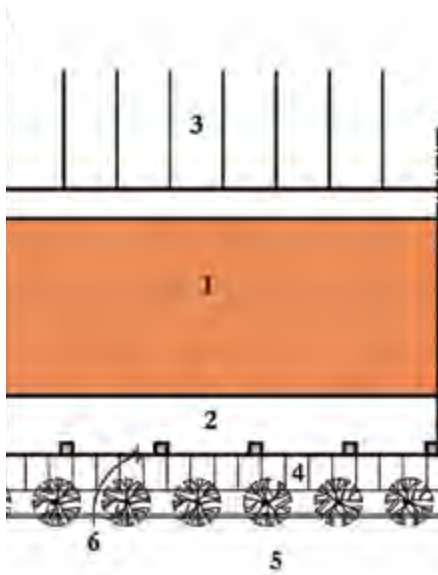


THE FINE GRAINED MULTIUSE DISTRICT

The fine grained multiuse district is intended to accommodate the general housing for **VICINIA** and as such will include product for all levels of the social spectrum represented by the richness of various price points, sensitively mixed. The mix will include various types of parks for use by all ages. The scale will begin the transition from the edges of lower buildings into the much more dense centers encouraged in the Plaza and Entry Districts.

THE EDGE DISTRICT

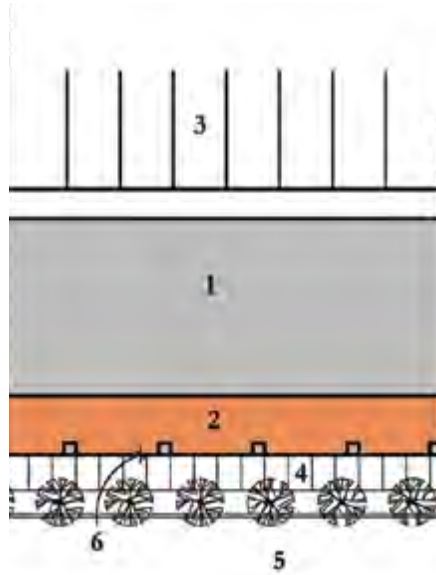
To be responsive to the land adjacent to **VICINIA**, the Edge district intentionally positions along its edge housing of similar ownership and scale. With townhouses, and mansion condos characterizing the building types, the Edge district is the finest grained neighborhood space offered in **VICINIA** with building heights lowered to 2 to 2.5 stories. While still physically connected to all aspects of **VICINIA** through a network of pedestrian friendly streets, passages and parks, its character is intentionally more informal and quaint.



BUILDING PLACEMENT

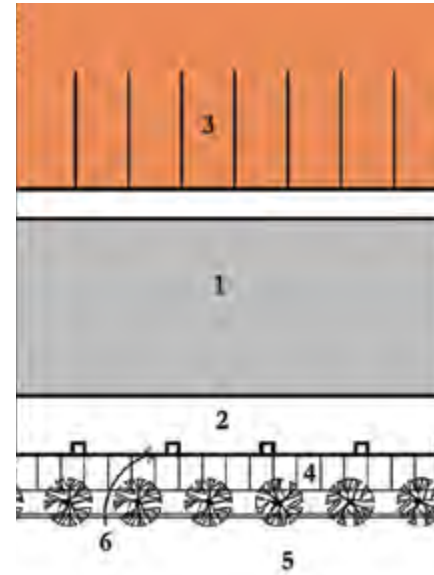
Buildings shall be placed with the shaded area as shown in the above diagram and urban regulation.

See urban regulation for setback notes on Facade, Fences and Garden Walls, Corner Lot, and Parking



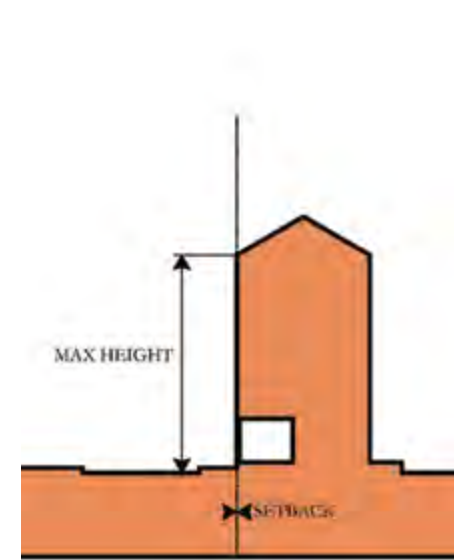
BUILDING FRONTAGE

Porches, Balconies, and Stoops shall be provided in any one of the combinations shown on Architectural Typologies.



PARKING PLACEMENT

On-site parking is allowed only in the shaded area as shown above. Vehicular access will be provided in urban regulation.



HEIGHT REQUIREMENTS

Building height shall be measured in number of stories. See Urban Regulation for heights of Porches, Balconies, Stoops, Main Floor Height, and Maximum Building Height.

Min Stories: See Urban Regulation
Max Stories: See Urban Regulation

LEGEND

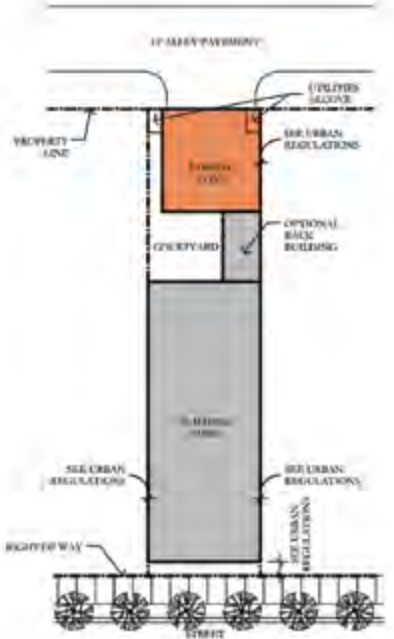
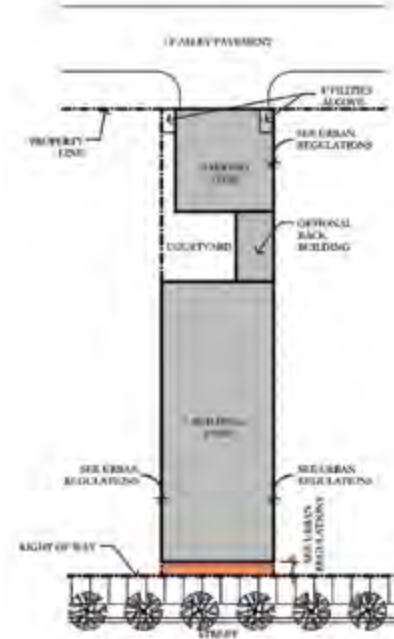
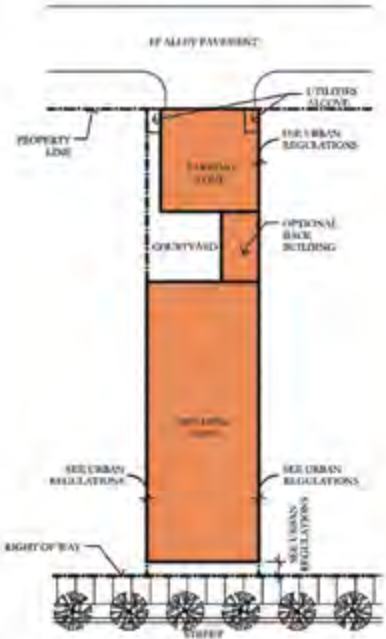
1. Building Zone
2. Optional Front Gallery or Awning Covering
3. Rear Parking Zone
4. Sidewalk
5. Street
6. See Urban Regulations

URBAN CENTER (T5)

The Urban Center (T5) is the focus of the neighborhood's civic buildings and social activity. It incorporates retail, workplaces, and more dense residential units in accordance with the specific use plan, and it connects directly to other parts of the neighborhood through a network of carefully articulated vehicular and pedestrian thoroughfares. As such, it is the densest graining of land subdivision in the district. The streets are generally designed with formalized on-street parking characterized by avenues and main streets. Buildings placed either at or near the right-of-way line, further reinforce the streets edge and public character.

TOWNHOUSES (T4)

The General Urban Zone (T4) is the least dense part of the community and includes townhomes, *VICINIA's* only single-family product.



BUILDING PLACEMENT

Buildings shall be placed with the shaded area as shown in the above diagram and urban regulation.

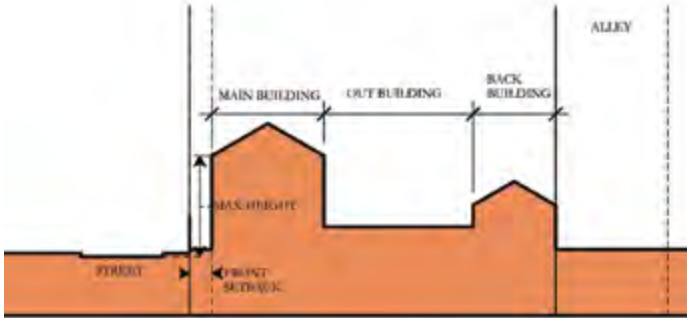
See urban regulation for setback notes on Facade, Fences and Garden Walls, Corner Lot, and Parking

ENCROACHMENTS

Porches, Balconies, and Stoops shall be provided in any one of the combinations shown on Architectural Typologies.

PARKING PLACEMENT

On-site parking is allowed only in the shaded area as shown above. Vehicular access will be provided in urban regulation.



HEIGHT REQUIREMENTS

Building height shall be measured in number of stories. See Urban Regulation for heights of Porches, Balconies, Stoops, Main Floor Height, and Maximum Building Height.

Min Stories: See Urban Regulation
Max Stories: See Urban Regulation













THOROUGHFARE STANDARDS

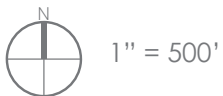
THOROUGHFARE PLAN

Based on the T4 and T5 context classifications, transportation facilities are planned to help achieve the mixed-use, walkable patterns essential to the TOD community vision. The function of these streets goes beyond the typical suburban arterial and collector streets which emphasize vehicle mobility and land access respectively. Both mobility and access are vital to all streets in the **VICINIA** TOD.

Mobility for all modes is the fundamental design assumption. Pedestrian, bicycle and transit modes are as vital as motor vehicle movement. Generous sidewalks, narrow lanes, curbside parking, street trees and build to lines for structures are all important to achieving greater walkability, primarily through vehicular speed management. Although the Boulevards and Commercial Streets are larger than the Streets, Yield Streets and Lanes, all streets are almost equal in their functions of providing mobility and access.

Access to all land uses from the edge of each street is also important. Parallel parking on most street edges allows drivers to park, and, within a reasonable walk, reach the building doors connecting them to their destinations. Pedestrians, given much greater advantage in the network, can move more easily and safely on multiple paths and gain essential access to buildings at the back of sidewalks that line all streets.

- | | |
|--|--|
|  PL |  ST 60-36 |
|  LA 30-14 |  CS 60-36 |
|  NS 36-20 |  CS 60-36 |
|  BV 39-24 |  CS86-48 |
|  YS 50-30 |  BV 98-68 |

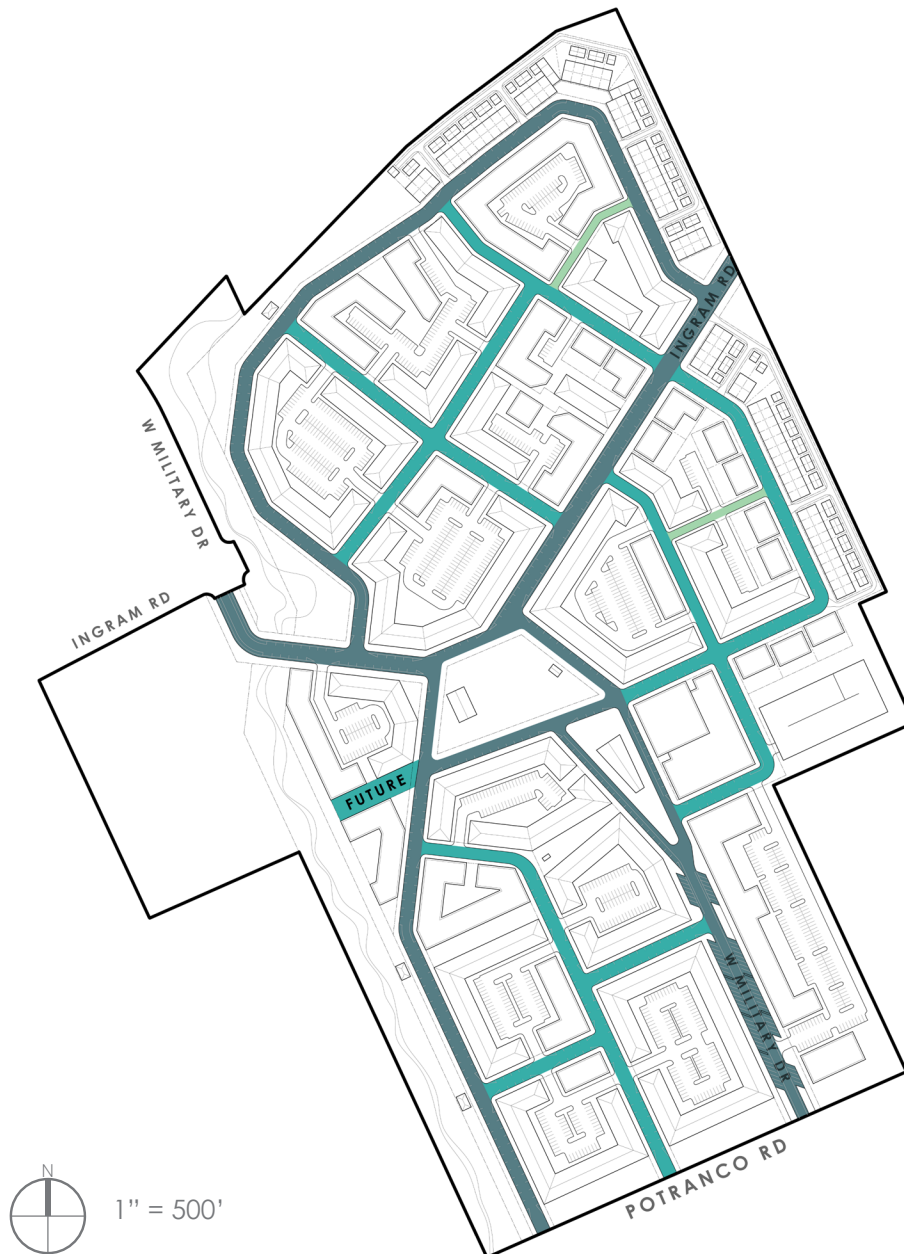


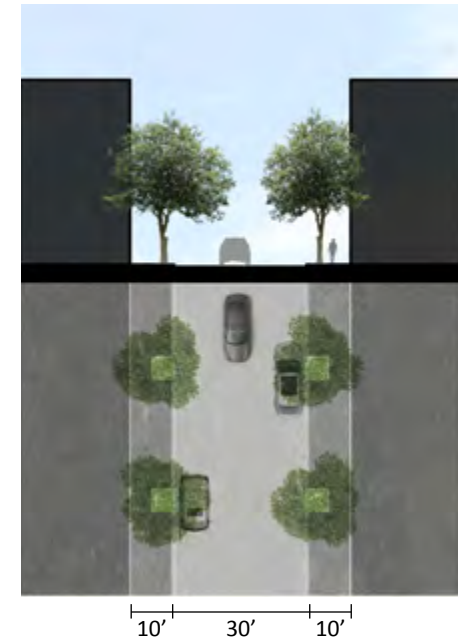
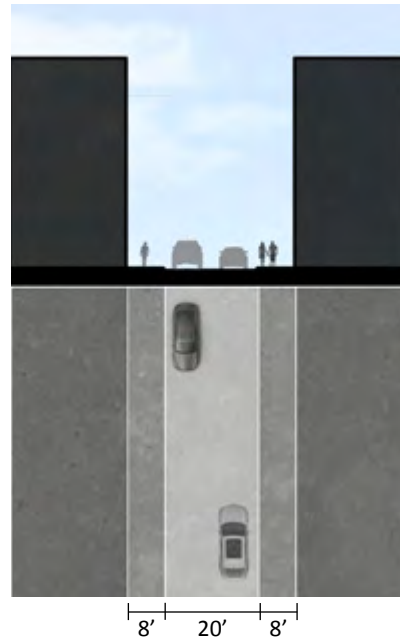
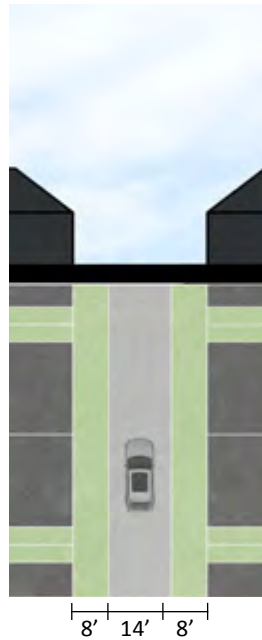
THOROUGHFARE CLASSIFICATIONS

The street network is essential to effective multi-modal travel within, to and through the town. Streets fulfill the vision urban designers begin as plans emerge. One classification for streets emphasizes the highly walkable, bikeable and transit friendly streets. These are deemed “A” Streets. The most commercial and residential are mixed via great urban design. The “B” streets are still walkable and mixed use, but fewer urban design features are applied to their design. They are simpler and less expensive to construct. Finally, the “C” Streets form vital but smaller pedestrian ways and may even be simple alleys.

The primary street design planning step is to place the correct streets in the correct location to facilitate the overall future town vision. Tree planting is more formal, in tree grates/planters at the town center (T5), less so at the edges (T4). Sidewalks in T5 are wider to accommodate the larger pedestrian volumes. All of the dozen features change based on context envisioned by experienced urban designers. The map to the left shows conceptually where each unique street will be constructed. Design will be refined as details emerge, but walkability will remain a primary design policy, thus requiring speed management.

-  A STREETS
-  B STREETS
-  C STREETS





PL

Transect	T5
Type	Pedestrian Lane
Movement	Pedestrian
Traffic Lanes	None
Parking Lanes	None
Bike Facility	Shared
Total Pavement Width	None
Median	None
Sidewalk Width	20 ft. Minimum
Planter	None
Total R.O.W.	20 ft. Minimum
Curb Radius	NA
Design Speed	None
Road Edge Treatment	None

LA 30-14

Transect	T4
Type	Lane
Movement	Yield
Traffic Lanes	Yield @ 14 ft.
Parking Lanes	None
Bike Facility	Shared
Total Pavement Width	14 ft. Plus 8 ft. Pervious Base Per Side
Median	None
Sidewalk Width	None
Planter	None
Total R.O.W.	30 ft.
Curb Radius	Apron
Design Speed	15 MPH
Road Edge Treatment	Inverted Crown with Header Curb

(A combination of single and double-loaded conditions may exist)

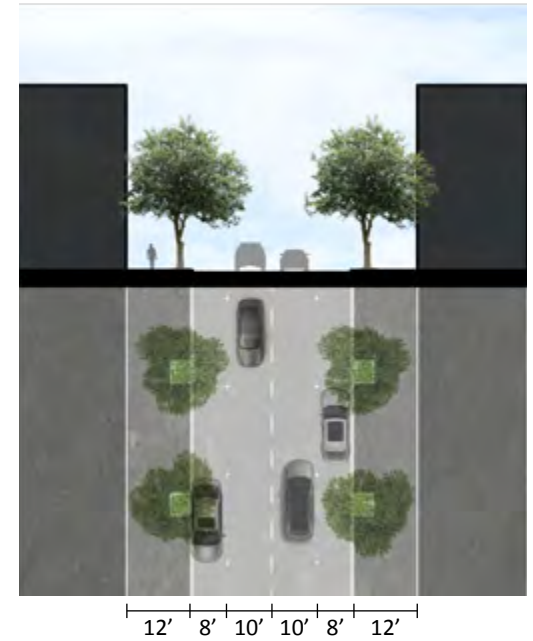
NS 36-20

Transect	T4, T5
Type	Narrow Street
Movement	Slow
Traffic Lanes	Two @ 10 ft.
Parking Lanes	None
Bike Facility	Shared
Total Pavement Width	20ft.
Median	None
Sidewalk Width	8 ft.
Planter	None
Total R.O.W.	36 ft.
Curb Radius	9 ft.
Design Speed	15 MPH
Road Edge Treatment	Curb

YS 50-30

Transect	T4, T5
Type	Yield Street
Movement	Yield
Traffic Lanes	Yield @ 15 ft.
Parking Lanes	Both Sides Informal
Bike Facility	Shared
Total Pavement Width	30 ft.
Median	None
Sidewalk Width	10 ft.
Planter	5x5 ft. Tree Well
Total R.O.W.	50 ft.
Curb Radius	9 ft.
Design Speed	15 MPH
Road Edge Treatment	Curb

Transect	T4, T5
Type	Yield Street
Movement	Yield
Traffic Lanes	Yield @ 15 ft.
Parking Lanes	Both Sides Informal
Bike Facility	Shared
Total Pavement Width	30 ft.
Median	None
Sidewalk Width	10 ft.
Planter	5x5 ft. Tree Well
Total R.O.W.	50 ft.
Curb Radius	9 ft.
Design Speed	15 MPH
Road Edge Treatment	Curb



ST 60-36

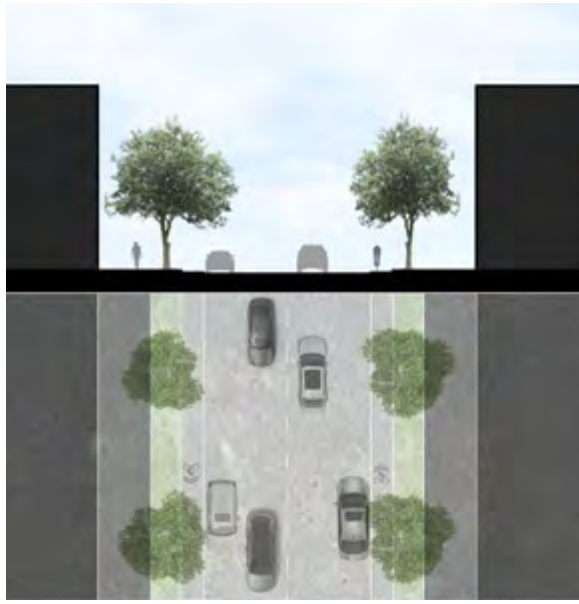
Transect	T4, T5
Type	Street
Movement	Free Movement
Traffic Lanes	Two @10 ft.
Parking Lanes	Both Sides @ 8 ft. Marked
Bike Facility	Shared
Total Pavement Width	36 ft.
Median	None
Sidewalk Width	6 ft.
Planter	6 ft. Green Strip
Total R.O.W.	60 ft.
Curb Radius	14 ft.
Design Speed	25 MPH
Road Edge Treatment	Curb

CS 60-30

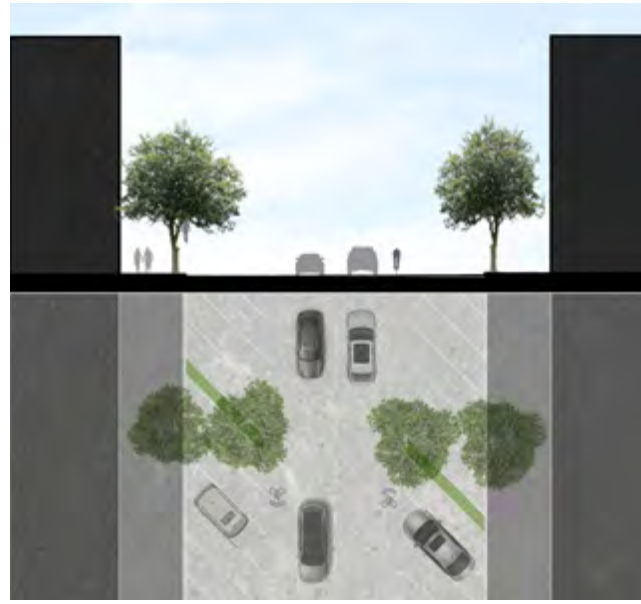
Transect	T5
Type	Commercial Street
Movement	Free Movement
Traffic Lanes	One-Way, Two @11 ft.
Parking Lanes	One Side @ 8 ft. Marked
Bike Facility	Shared
Total Pavement Width	30 ft.
Median	None
Sidewalk Width	15 ft.
Planter	5x5 ft. Tree Well
Total R.O.W.	60 ft.
Curb Radius	14 ft.
Design Speed	25 MPH
Road Edge Treatment	Curb @ Parking, No Curb at Plaza

CS 60-36

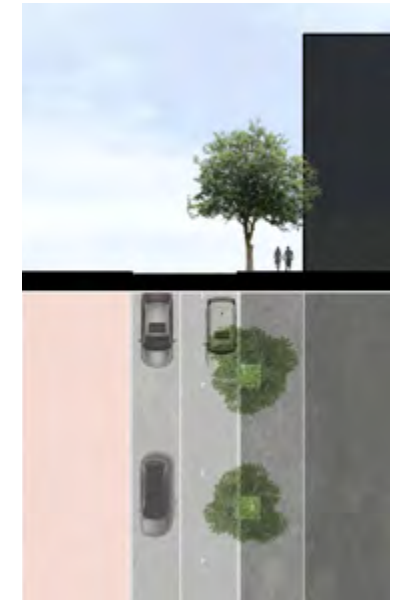
Transect	T5
Type	Commercial Street
Movement	Free Movement
Traffic Lanes	Two @10 ft.
Parking Lanes	Both Sides @ 8 ft. Marked
Bike Facility	Shared
Total Pavement Width	36 ft.
Median	None
Sidewalk Width	12 ft.
Planter	5x5 ft. Tree Well
Total R.O.W.	60 ft.
Curb Radius	14 ft.
Design Speed	25 MPH
Road Edge Treatment	Curb



12' 7' 5' 8' 11' 11' 8' 5' 7' 12'



15' 18' 5' 11' 11' 5' 18' 15'



11' 5' 8' 15'

CS 86-48

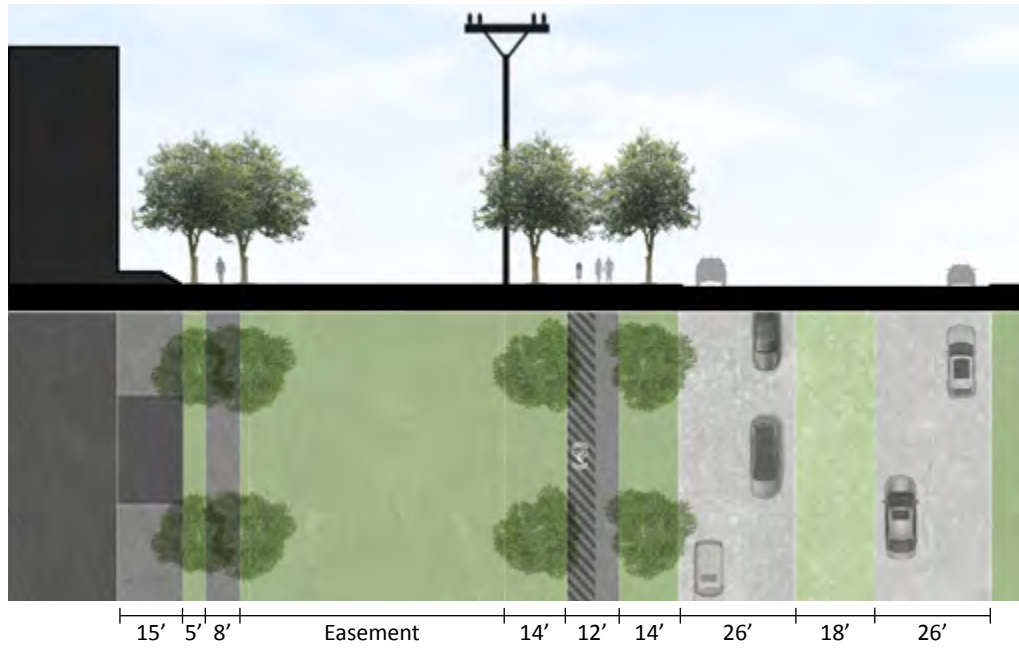
Transect	T4, T5
Type	Commercial Street
Movement	Free Movement
Traffic Lanes	Two @11 ft.
Parking Lanes	Both Sides @ 8 ft. Marked
Bike Facility	Two Bike Lanes @ 5 ft. Marked
Total Pavement Width	48 ft.
Median	None
Sidewalk Width	12 ft.
Planter	7 ft. Green Strip
Total R.O.W.	86 ft.
Curb Radius	14 ft.
Design Speed	25 MPH
Road Edge Treatment	Curb

BV 98-68

Transect	T5
Type	Boulevard
Movement	Free Movement
Traffic Lanes	Two @11 ft.
Parking Lanes	Both Sides @ 18 ft. Angled
Bike Facility	Two Bike Lanes @ 5 ft. Marked
Total Pavement Width	68 ft.
Median	None
Sidewalk Width	15 ft.
Planter	5x5 ft. Tree Well
Total R.O.W.	98 ft.
Curb Radius	14 ft.
Design Speed	25 MPH
Road Edge Treatment	Curb

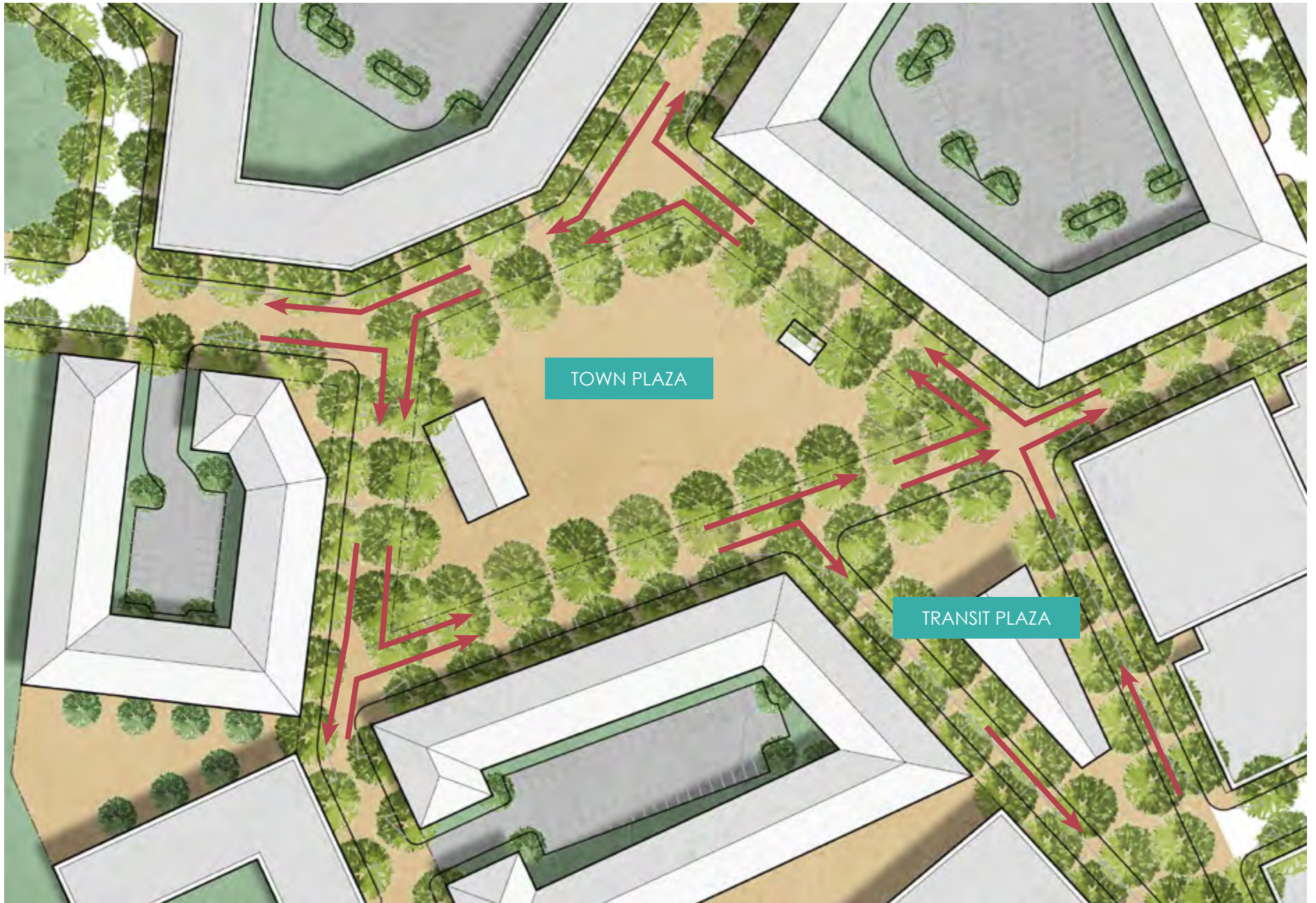
BV 39-24

Transect	T5
Type	Boulevard
Movement	Free Movement
Traffic Lanes	One @11 ft.
Parking Lanes	One Side 8 ft. Marked
Bike Facility	One Lane @ 5 ft. Marked
Total Pavement Width	24 ft.
Median	None
Sidewalk Width	15 ft.
Planter	5x5 ft. Tree Well
Total R.O.W.	39 ft.
Curb Radius	9 ft.
Design Speed	25 MPH
Road Edge Treatment	Curb



POTRANCO (PROPOSED)

Transect	T5
Type	Boulevard
Movement	Free Movement
Traffic Lanes	Four @13 ft.
Parking Lanes	None
Bike Facility	Path
Total Pavement Width	70 ft.
Median	18 ft.
Sidewalk Width	12 ft.
Planter	14 ft. Green Strip
Total R.O.W.	-
Curb Radius	14 ft.
Design Speed	30 MPH
Road Edge Treatment	Curb





Aerial view of the **VICINIA** town plaza.

THE TURBINE

The vibrant core of **VICINIA** lies within the centrally located town plaza. This town center will be enclosed by dense residential and bustling commercial activity. Commercial streets will be utilized to frame the town center. The configuration of these streets will create what is referred to as a turbine.

The turbine is composed of an eight foot strip of formal parallel parking along the commercial building frontages. Adjacent to the on-street parking are two, one-way travel lanes. Both travel lanes move vehicles in a counter-clockwise direction around the town plaza. The turbine plaza at the town center is designed to have all vehicles pause before entering. This design feature helps manage speeds to levels comfortable for pedestrians. Scale is also set to suit the walkers and cyclists and keep the drivers moving at reasonable flow rates.

An urban bosque in conjunction with bollards is being utilized around the perimeter of the town plaza. This urban bosque will create a distinct separation between the turbine traffic flow and the pedestrian-only component of the plaza.

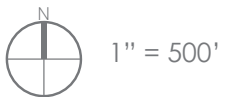
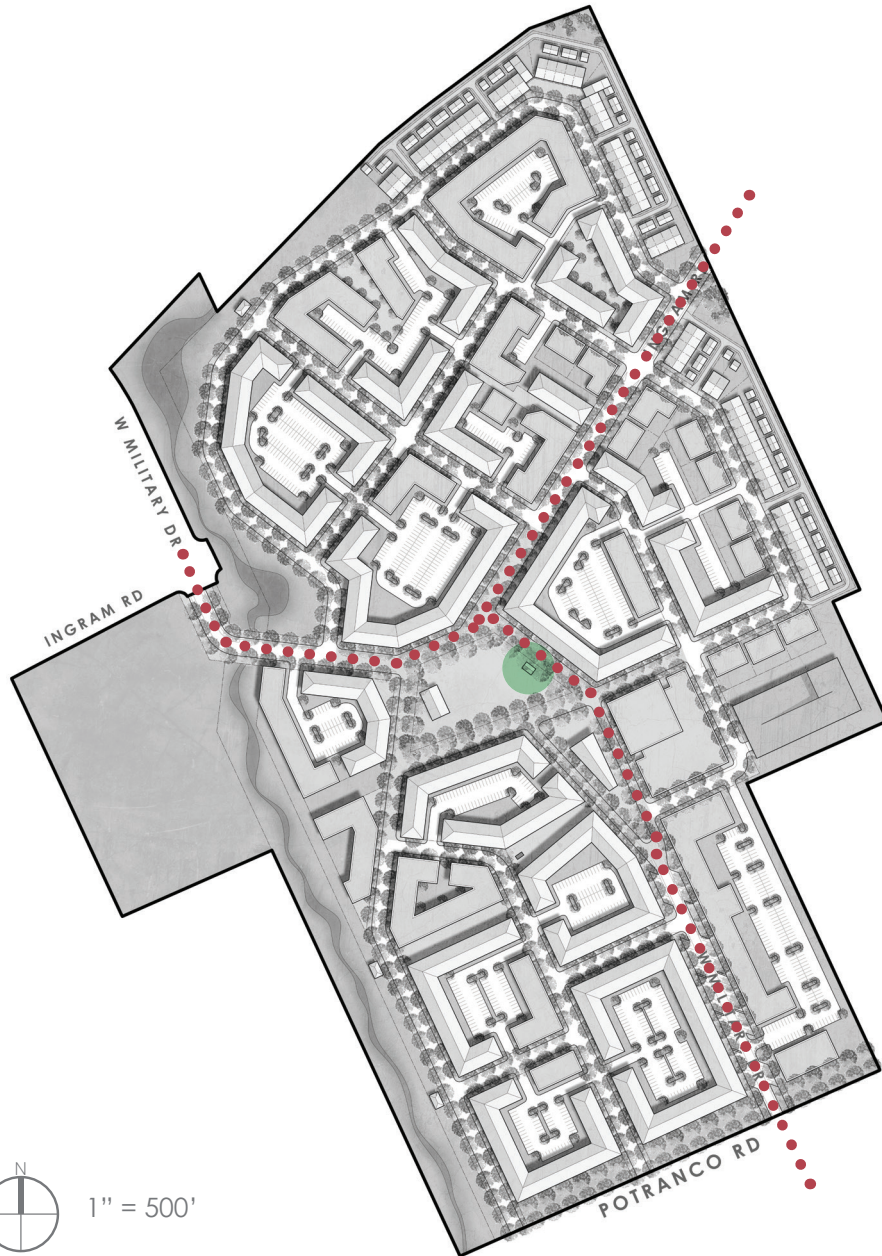


An urban bosque at the Christian Science Center in Boston. A design similar to this is intended to surround the perimeter of the **VICINIA** town plaza.

RAPID TRANSIT ROUTE

The dotted line in the diagram to the left indicates the proposed transit routes through **VICINIA**. Both Ingram Road and W Military Drive are intended to be utilized for bus transit with the possibility of light rail service being added in the future. A transit stop, highlighted in green, has been located within the **VICINIA** town plaza. The transit stop is centrally located within a five minute walk of nearly the entire community.

- TRANSIT ROUTE
- TRANSIT STOP



PEDESTRIAN NETWORK



Within the more urban transect zones (T4 through T6), pedestrian comfort shall be a primary consideration of the thoroughfares. Design conflicts between vehicular and pedestrian movement shall be decided in favor of the pedestrian.

Trees provide many functions to aid the mobility and safety of travelers. Cooler shaded places for walking, dining and other gathering activity are greatly enhanced by street trees. The silent uptake of CO2 helps cleanse the air along street edges and the town in general. Visual sensations of regularly spaced tree trunks passing the driver's eye afford a clear, rhythmic feedback on vehicle speed, allowing drivers to adjust speed to match urban conditions. Trees also shade parked vehicles and surrounding pavement, countering the heat effect of urban hardscapes.



FORMAL/INFORMAL PARKING

Parking is designed for almost every street in walkable places. For individuals not able to live within town boundaries, a short or long drive is accommodated with sufficient spaces in one of the many on-street spaces or in eventual garages. Both sides of most streets are parked, based on the planned combination of mobility and access. The thoroughfares adapted and designed for **VICINIA** are drawn from streets we have designed and built in the past. We know they work.

-  FORMAL PARKING
-  INFORMAL PARKING

